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Section 16 Planning Application

Proposed Temporary Warehouse for Storage of Construction Materials (Metal) for a Period of 3 Years at Lots 1364 S.B RP and 1364 S.B ss.1 RP in D.D. 78, Lin Ma Hang Road, Ta Kwu Ling, N.T.

Planning Statement

Applicant

Hang Sing Limited (行陸有限公司)

Prepared by

Lawson David & Sung Surveyors Limited

April 2024

Executive Summary

This planning statement is submitted to the Town Planning Board (the “Board”) in support of a planning application for Proposed Temporary Warehouse for Storage of Construction Materials (Metal) for a Period of 3 Years at Lots 1364 S.B RP and 1364 S.B ss.1 RP in D.D. 78, Lin Ma Hang Road, Ta Kwu Ling, N.T. (“the Application Site”).

The Application Site, covering an area of about 1,105 sq.m., falls within an area zoned “Recreation” (“REC”) on the Approved Ta Kwu Ling North Outline Zoning Plan (OZP) No. S/NE-TKLN/2 gazetted on 13.5.2016. According to the Notes of the OZP, ‘Warehouse’ is neither a Column 1 nor Column 2 use of the “REC” zone, and temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board.

The Applicant is currently operating at Yuen Long with their business of construction materials (metal), machinery, vehicles parts and electronic goods since early 2000s. They are one of the business operators affected by the land resumption for Second Phase Development of Yuen Long South Development Area. The Applicant is informed that they are required to relocate before September 2024. The Applicant has identified the Application Site as a possible site for relocation and decide to relocate their operation at Lot 1414 in D.D. 119, which is currently used as a warehouse for storage of construction materials (metal), and same as the applied use at the Application Site.

The proposed development will erect a 1-storey warehouse for storage of construction materials, with a floor area of about 607 sq.m. and height of not more than 13m at the northern portion of the Application Site. The southern portion will be used as an 1-storey (height: not more than 13m) open-sided shelter with an area of about 276 sq.m. for loading/unloading activities and manoeuvring of vehicles near the ingress/egress point. An 1-storey (height: about 2.5m) guard room of 2 sq.m. will be erected next to the proposed ingress/egress point at the southern boundary. The Applicant will widen the existing ingress/egress point to 8m in width at the southern boundary connecting Lin Ma Hang Road.

There will be 1 loading/unloading space (3.5m x 11m) for medium goods vehicle, and 2 private car parking spaces for staff/visitors (2.5m x 5m). The operation hours of the Application Site are between 9am to 7pm from Mondays to Saturdays. There will be no operation on Sundays and public holidays. To minimize any potential noise and environmental impacts to the surrounding area, the proposed warehouse will be enclosed with no workshop activities conducted within the Application Site.

The justifications of this application are:

1. The proposed development helps support the construction industry in Hong Kong;
2. The proposed development is in line with the future planning intention of Northern Metropolis;
3. The Application Site is the best location choice for the Applicant;
4. The proposed development does not contravene the planning intention of “REC” zone;
5. Similar application is approved within the same “REC” zone;
6. The proposed development allows optimization of valuable land resources;
7. The proposed development is not incompatible with surrounding land uses;
8. The proposed development will not generate adverse traffic, environmental and drainage impact on the surrounding areas;
9. The proposed development will not create undesirable precedent.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 3 years.

行政摘要

此規劃報告書向城市規劃委員會(“城規會”)呈交，旨在支持在新界打鼓嶺蓮麻坑路丈量約份第 78 約地段第 1364 號 B 分段餘段及第 1364 號 B 分段第 1 小分段餘段(“申請地點”)，作為期三年的臨時存放建築材料(五金物料)貨倉(“擬議發展”)的規劃申請。

申請地點的面積約 1,105 平方米，座落於在 2016 年 5 月 13 日刊憲的打鼓嶺北分區計劃大綱核准圖(圖則編號：S/NE-TKLN/2)上的「康樂」地帶。根據該大綱圖的註釋，「貨倉」不屬「康樂」地帶內的第一欄或第二欄用途，然而不超過三年土地或建築物的臨時用途或發展，須先向城規會提出申請。

申請人自 2000 年代初開始於元朗設置貨倉，用作存放建築材料(五金物料)、機械、汽車零件及電子產品。申請人是受元朗南發展區第二期發展計劃收地影響的營運者之一，並收到通知需於 2024 年 9 月前搬遷。

申請人已覓得申請地點作為重置貨倉地點，決定重置現時於新界元朗丈量約份第 119 約地段第 1414 號存放建築材料的貨倉到申請地點，以繼續營運。

擬議發展將於申請地點北面興建一個樓面面積約 607 平方米以及其高度不超過 13 米的一層高貨倉，用作存放建築材料。同時，於申請地點南面設置一個樓面面積約 276 平方米以及其高度不超過 13 米的一層高開放式上蓋，以便上落貨及車輛迴轉。此外，申請地點南邊近出入口亦會提供一座一層高(約 2.5 米高)，面積約 2 平方米的保安室。擬議發展將擴闊現時於場地南邊的出入口至 8 米，連接蓮麻坑路。申請地點亦設有 1 個中型貨車上落貨位(3.5 米 x 11 米)和 2 個供職員/訪客使用的私家車停車位(2.5 米 x 5 米)。擬議發展的營運時間為星期一至星期六上午 9 時至下午 7 時，星期日及公眾假期不會運作。為減少擬議發展對周遭的潛在噪音及環境影響，擬議貨倉是以密封形式興建，以及於申請地點內不會進行任何工場活動。

本規劃申請的理據為：

1. 擬議發展有助支持本港的建造業；
2. 擬議發展合乎北部都會區未來的規劃意向；
3. 申請地點是申請人搬遷的最佳位置選擇；
4. 擬議發展不會違背「康樂」地帶的規劃意向；
5. 申請地點座落的「康樂」地帶已有類近申請獲得規劃許可；
6. 擬議發展可優化申請地點及有效利用寶貴的土地資源；
7. 擬議發展與周邊土地用途兼容；
8. 擬議發展不會對附近地方構成不良的交通、環境及排水影響；及
9. 擬議發展不會被定為不良的先例。

基於本規劃報告書所述的理據，敬希 各城規會委員及有關政府部門能批准此項申請，作為期三年的臨時用途。

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1. Introduction

1.1 Purpose

Pursuant to Section 16 of the Town Planning Ordinance, this Planning Statement Report is submitted to the Town Planning Board (“the Board”) in support of a planning application for Proposed Temporary Warehouse for Storage of Construction Materials (Metal) (the “proposed development”) for a period of 3 years at Lots 1364 S.B RP and 1364 S.B ss.1 RP in D.D. 78, Lin Ma Hang Road, Ta Kwu Ling, N.T. (hereafter referred to as “the Application Site”). **Figure 1** shows the location of the Application Site and the Lot Index Plan indicating the concerned lots is shown per **Figure 2**.

The Application Site, covering an area of about 1,105 sq.m., falls within an area zoned “Recreation” (“REC”) on the Approved Ta Kwu Ling North Outline Zoning Plan (OZP) No. S/NE-TKLN/2 gazetted on 13.5.2016. According to the Notes of the OZP, ‘Warehouse’ is neither a Column 1 nor Column 2 use of the “REC” zone, and temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board.

1.2 Background of the Application

Background of the Applicant

The Applicant, **Hang Sing Limited** (hereafter “the Applicant”), has been operating their business of construction materials (metal), machinery, vehicles parts and electronic goods in Yuen Long since the early 2000s, in which they supply assorted goods to different construction companies and hardware stores in the market.

The Applicant is currently operating at various lots in D.D. 119 and D.D. 121 at Shan Ha Tsuen, Yuen Long, N.T. with multiple open storage and warehouses in the locality (see **Appendix A**). Since the area is subject to land resumption for the Second Phase development of Yuen Long South Development Area, the Applicant is informed by the Lands Department that their existing warehouses would be under site clearance in September 2024 and they are required to relocate by then.

The Applicant intends to continue their operation after the site clearance of their existing operations in Yuen Long and is required to identify a suitable site to relocate their operation to sustain their business. However, due to ongoing government land resumption schemes in the Northern Metropolis area, it is extremely difficult to search for suitable site of adequate size that can accommodate all of their existing operations at once. As such, the Applicant has decided to search for multiple relocation sites of smaller size to continue respective operation.

Given the limited time before the site clearance and scarce supply of suitable sites for warehouse operation within the budget of the Applicant, the Applicant has identified the Application Site and decided to relocate their existing operation at Lot 1414 in D.D. 119, Shan Ha Tsuen, Yuen Long, N.T., which is currently used as a warehouse for storage of construction materials (metal) to the Application Site. The current applied use is the same as the affected business in Yuen Long.

While the Application Site has a larger site in comparison with the Applicant's current operation site in Yuen Long, it should be noted that the Applicant is able to shift their stocks from warehouse to warehouse based on the seasonal demand and supply of different goods at their existing location in Yuen Long. As the Applicant has to now split up their business operation in different locations, the Applicant requires a larger site in order to store all the stocks for their operation needs.

In addition, while the Applicant has currently made use of their surrounding warehouse sites and open space for loading/unloading and manoeuvring of goods vehicles at the vacant land at the current operation site in Yuen Long, the same scenario is yet not feasible at the Application Site. Therefore, to avoid any goods vehicle queuing up outside the Application Site and causing any traffic at Lin Ma Hang Road, the southern portion of the Application Site has been reserved for an open-sided shelter to allow loading/unloading and smooth manoeuvring of goods and private vehicles.

Relocation Need of the Applicant

With the ongoing land resumption in the locality of Yuen Long South, operators of storage yards / warehouses in the area might be forced to terminate their business after land resumption due to lack of compatible alternative land in the market. Therefore, to sustain and continue their business, it is necessary for the Applicant to be able to relocate their business and continue to contribute to the construction industry in Hong Kong.

Various discussions were made between the Development Bureau and the Applicant regarding the relocation of the Applicant's warehouse. Due to the limited time before the clearance of their existing site in Yeun Long, the Applicant is required to identify a suitable site that can fulfill their operational needs within the current timeframe of September 2024.

However, the Applicant has faced tremendous difficulties searching for a suitable site in the New Territories. The ongoing land resumption schemes in the Northern Metropolis area has caused a stiff competition for suitable sites for storage yards / warehouses. It is difficult for the Applicant to identify suitable site for relocation as land within the Categories 1 and 2 areas in the region under TPB PG-No. 13G was either occupied by other open storage/storage uses or over the price consideration of the Applicant. Meanwhile, taken into consideration that the storage materials are bulky in nature which involves considerable land requirements i.e. site with considerable size, appropriate access road for long vehicles, etc., the Applicant struggles to identify possible sites for relocation.

The Applicant has conducted a thorough site search exercise before identifying the Application Site as the best available option for relocation. The Applicant has investigated about 4 possible sites located in assorted areas in New Territories such as Ta Kwu Ling, Kwu Tung South, Tai Po and Ping Che (see **Appendix B**), in which all require planning permission for warehouse use under section 16 of the Town Planning Ordinance (TPO). All are considered not suitable for relocation due to various reasons, for instance, geographical constraints, unaffordable potential construction costs, potential right of way disputes from private lots owners, perceived local objections and traffic concerns.

As a result, the Applicant has identified the Application Site as the best available option for a number of reasons. Firstly, the Application Site is directly accessible from Lin Ma Hang Road, which

provides good accessibility for the staff to commute to work, and allow the various sizable goods vehicles to access to the site. Secondly, the Application Site is currently vacant with most of the site being formed already. A swift transformation into a warehouse for the Applicant's imminent usage is thus feasible. Last but not least, the size of the Application Site could meet the immediate need of the Applicant to accommodate their business operation with sufficient manoeuvring space. The Applicant has decided choosing the Application Site for relocation taking into account that the availability of similar sites in the market is limited.

As the basic settings of Application Site meet the operational needs of the Applicant, the Applicant seeks the Board's permission to use the Application Site as a temporary warehouse for storage of construction materials (metal) for relocation of their business. The proposed development will also allow better utilization of scarce land resources so that the land can be under better use.

Following this introductory chapter, this planning statement contains five further chapters:

- Chapter 2 presents the background details of the Application Site in terms of current land-use characteristics and neighbouring developments;
- Chapter 3 discusses the planning context of the Application Site;
- Chapter 4 outlines the development proposal;
- Chapter 5 gives account to the justifications of the proposed development;
- Chapter 6 is the conclusion of the planning statement.

2. Site Context

2.1 The Application Site and Its Existing Condition

The Application Site, with an area of about 1,105 sq.m., abuts Lin Ma Hang Road near Heung Yuen Wai Border Control Point (BCP) in Ta Kwu Ling, N.T. The Application Site is generally flat, formed and vacant. The Application Site is fenced off by 2.5m high corrugated metal sheets (see **Photo Plate 1**).

2.2 Surrounding Land Use

The surrounding area of the Application Site is rural in nature and consists of vacant land, temporary structures, open storage and village settlements. The Application Site is surrounded by vacant land at the immediate north, east and west. To the south across Lin Ma Hang Road sees mostly vacant land intermixed with several open storage/storage yards. To the further north of the Application Site sees scattered temporary structures and the Heung Yuen Wai BCP across Heung Yuen Wai Highway. Meanwhile, to the further south of the Application Site along Lin Ma Hang Road finds a fenced open storage site and the village settlements of Kaw Liu Village.

2.3 Accessibility

The Application Site enjoys good accessibility to strategic road network. It has direct access to Lin Ma Hang Road, which can be connected to Heung Yuen Wai Highway, and provide quick access to the Heung Yuen Wai BCP to Mainland China. It is also conveniently connected to other parts of the New Territories via Heung Yuen Wai Highway. A plan showing the vehicular access to the Application Site is provided at **Figure 3**.

2.4 Land Status

According to the records of Land Registry, the Subject Lots are held under Block Government Lease and demised for agricultural use with a lease term for 75 years, commenced from 1.7.1898, and are renewable for a further term of 24 years.

The Subject Lots are old schedule agricultural lots. The Applicant will apply for a Short Term Waiver (STW) for the proposed structures to the Lands Department upon obtaining planning approval for this application.

3. Planning Context

3.1 Outline Zoning Plan

The Application Site falls within an area zoned "Recreation" ("REC") on the Approved Ta Kwu Ling North Outline Zoning Plan (OZP) No. S/NE-TKLN/2 gazetted on 13.5.2016 (see **Figure 1**). The planning intention of "REC" zone is "primarily for low-density recreational developments for the use of the general public. It encourages the development of active and/or passive recreation and tourism/eco-tourism. Uses in support of the low-density recreational developments may be permitted subject to planning permission."

Despite the fact that the proposed development ('warehouse') is neither one of the Column 1 or Column 2 uses under the Schedule of Uses for the subject "REC" zone, the Board may grant

planning permission (with or without conditions) for a maximum period of three years under section 16 of the TPO.

3.2 Previous Application

The Applicant Site is not subject to any previous planning application.

3.3 Similar Application

The Board has recently approved a planning application of similar warehouse uses in the same “REC” zone as below:

Application No.	A/NE-TKLN/77
Applied Use	Temporary Logistic Centre, Warehouse (Excluding Dangerous Goods Godown) and Container Vehicle Park with Ancillary Facilities for a Period of 3 Years
Location	Various Lots in D.D. 78 and 82 and Adjoining Government Land, Lin Ma Hang Road, Ta Kwu Ling North, N.T.
Site Area	About 122,819 sq.m.
Gross Floor Area	About 130,848 sq.m.
No. of Block	6
Building Height	2 storeys (7 – 15m)
TPB’s Decision	Approved with Conditions for a period of 3 years on 15.3.2024

4. Development Proposal

4.1 Site Configuration and Layout

It is proposed to use the Application Site as temporary warehouse for storage of construction materials (metal) for a period of 3 years. The Application Site has an area of about 1,105 sq.m.

The proposed 1-storey warehouse, with a floor area of about 607 sq.m. and height of not more than 13m, will be erected for the storage of construction materials (metal) at the northern portion of the Application Site. The southern portion will then be used as an open-sided shelter with an area of about 276 sq.m. for loading/unloading activities and manoeuvring of vehicles near the ingress/egress point. An 1-storey (height: about 2.5m) guard room of about 2 sq.m. will be erected next to the proposed ingress/egress point at the southern boundary (see Layout Plan at **Figure 4**).

The Application Site will be continued to be fenced off by the existing 2.5m high corrugated metal sheets on all sides. To minimize any potential noise and environmental impacts to the surrounding area, no workshop activities will be conducted within the Application Site.

4.2 Vehicular Access and Parking Arrangement

The Applicant will widen the existing ingress/egress point of the Application Site to about 8m wide at the southern side of the Application Site connecting to Lin Ma Hang Road. The run-in/out will be constructed by the Applicant according to Highways Department (HyD)’s standard drawings Nos. H1113C and H1114B. Meanwhile, the design of the drop kerb will follow HyD’s Transport Planning

and Design Manual (TPDM), Volume 2, Chapter 3.6, Diagram 3.6.3.1 (see Drop Kerb Details and HyD typical details of run-in at **Appendix C**).

The Applicant will provide 1 loading/unloading space (3.5m x 11m) for medium goods vehicle and 2 private car parking spaces (2.5m x 5m) for staff/visitors. A swept path analysis (see **Appendix D**) is prepared to demonstrate that satisfactory manoeuvring of vehicles entering to and exiting from the Application Site and manoeuvring within the Application Site. There will be no difficulties in internal traffic circulation sense as sufficient space for manoeuvring of vehicles is reserved at the southern portion such that no waiting or queuing of goods vehicles along Lin Ma Hang Road will arise under any circumstances. In addition, according to the Applicant, trips for transporting the construction materials to/from the Application Site made by the goods vehicles will not be more than 3 round trips daily at non-peak hours (between 10am – 4pm) with the materials stored within the Application Site remaining stagnant all the time. There will be no more than 5 working staff on-site. Considering the business nature and operational needs, staff can take public transport to the Application Site and the traffic generated by private vehicles (staff) can be minimized. The estimated traffic arising from the proposed development is as follows:

Time	No. of Trips (Medium Goods Vehicles)		No. of Trips (Private Cars)	
	In	Out	In	Out
08:00-09:00 (peak hour)	0	0	1	0
09:00-10:00 (peak hour)	0	0	1	0
10:00-11:00	1	0	0	0
11:00-12:00	1	1	0	0
12:00-13:00	0	1	0	0
13:00-14:00	0	0	0	0
15:00-16:00	1	1	0	0
16:00-17:00	0	0	0	0
17:00-19:00 (peak hour)	0	0	0	2

4.3 Site Operations

The operation hours of the Application Site are between 9am to 7pm from Mondays to Saturdays. There will be no operation on Sundays and public holidays. The main use of the Application Site will be storage purpose of construction materials (metal), which is same as the current use at the existing operation site in Yuen Long. To minimize any potential noise and environmental impacts to the surrounding area, the proposed warehouse will be enclosed with no workshop activities to be conducted within the Application Site.

4.4 Landscape and Tree Preservation

There are no existing trees within the Application Site. Meanwhile, existing trees of various species are found immediate outside the northern, western and eastern boundary of the Application Site. These existing trees will not be disturbed by the Applicant and would act as the natural hedges bounding the edge of the development and thus soften the building footprint of the proposed development. As a result, the proposed development would fit well into the setting in the neighborhood without causing any significant visual impact.

4.5 Drainage Proposal

No drainage facilities can be found within the Application Site while surface channels are found outside the southern boundary along Lin Ma Hang Road. Peripheral 225mm U-channels and catchpit with sand trap are proposed to collect and divert surface runoff from the Application Site to the u-channel outside the southern boundary of the Application Site. A drainage proposal is submitted (see **Appendix E**) to show the proposed drainage design.

4.6 Fire Service Installations Proposal

To minimize the fire hazard, the Applicant will provide fire service installations to the satisfaction of the Fire Services Department. A fire service installations proposal (see **Appendix F**) is submitted together with this application.

4.7 Environmental Consideration

The nature of the proposed use will merely involve the storage of construction materials. To minimize the possible environmental nuisance, the Applicant will follow the 'Code of Practice on Handling the Environmental Aspects of Temporary Uses and Open Storage Sites' and undertake to provide the following mitigation measures:

- (a) No workshop activities will be conducted at the Application Site;
- (b) All stocks will be stored within the proposed enclosed warehouse;
- (c) The existing 2.5m high corrugated metal fencing will be maintained along the site boundary;
- (d) The operating hours will be restricted from 9 am to 7 pm on Mondays to Saturdays and there will be no operation on Sundays and public holidays; and
- (e) No more than 3 round trips by goods vehicles daily to minimize the traffic and noise impact.

5. Planning Justifications

5.1 Support the Construction Industry in Hong Kong

With the ongoing resumption projects in the Northern Metropolis area, a significant number of operators in the construction industries is affected as their operation sites have been/will be resumed. As the Applicant is keen to continue their business, they have therefore searched for multiple smaller sites for relocating different operations, in which they require the Application Site as the warehouse for storage of construction materials.

As announced in the recent Policy Address, it is anticipated that there would be an increasing number of development projects in the coming years such as public works for various New Development Areas, new public housing sites and new transport systems. The Application Site could provide land for the proposed relocation of warehouse directly related to or in support of the public works of the Government and private developments. Therefore, the approval of this application would allow the Applicant to continue their business and support the construction industry in Hong Kong.

5.2 In Line with the Future Planning Intention of Northern Metropolis

With reference to the Northern Metropolis Action Agenda published in October 2023, the Application Site is within the “Boundary Commerce and Industry Zone”, in which the area near the Heung Yuen Wai BCP is intended for modern logistics development and upgrading of traditional industries. The proposed development, which is to support the Applicant’s business in terms of logistics arrangement of construction materials, would be in line with the future planning intention of logistics development of the area.

5.3 Best Location Choice of the Application Site

With the ongoing land resumption projects of the Northern Metropolis, it could be assumed that many private land in the Northern New Territories and North East New Territories will be resumed by the Government for development of NDAs/public works in the near future. Tremendous difficulties are encountered by the Applicant to identify a suitable site to continue their business with the lack of available site and financial constraints. Due to limited available land supply for warehouse and stringent timeframe before the clearance of the existing operation site, the only piece of land that meet the Applicant’s operational needs and could be secured by the Applicant is the current Application Site.

The Application Site is conveniently directly connected to Lin Ma Hang Road, which is connected to Heung Yuen Wai Highway and Heung Yuen Wai BCP. It allows easy transportation of construction materials from and to Mainland China with ease. The Application Site is also flat and formed, which allows prompt transformation into the proposed development for the operation of the Applicant. Meanwhile, the Application Site has been vacant for a long period of time. As such, the proposed use of the Application Site for warehouse for storage of construction materials is actually a practical and reasonable solution to the relocation demand of the Applicant. Being located close to a main road and having a compatible size to accommodate the operational needs of the Applicant, the Application Site is the best location choice.

5.4 Not Contravene the Planning Intention of “REC” Zone

The Application Site falls within an area zoned “REC” on the Ta Kwu Ling North OZP No. S/NE-TKLN/2. In view of the surrounding developments of the Application Site, which is mainly occupied by scattered temporary structures, open storage and unused vacant land, the planning intention of the “REC” zone has not been materialized. It should also be noted that there is no known long-term recreational development at the Application Site or its surrounding area within the applied planning period. Additionally, the proposed development is a temporary use which would not jeopardize the long-term planning intention of “REC” zone.

5.5 Similar Application approved within the same “REC” zone

As mentioned in Section 3.3, an application of similar warehouse uses (No. A/NE-TKLN/77) for relocation purpose within the same “REC” zone of the Ta Kwu Ling North OZP has been recently approved by the Board on 15.3.2024. The approval of this application indicates that the subject locality will be open to cater warehouse / logistics uses, which aligns with future planning intention of the locality under the Northern Metropolis Action Agenda. The proposed development, which is for similar temporary warehouse uses, could be considered tolerable by the Board.

5.6 Optimization of Valuable Land Resources

The Application Site is currently vacant. The proposed development provides an interim solution to maximize land utilization on the Application Site rather than leaving the Application Site idle and deteriorating. The proposed development would take full advantage of the location and the exceptional physical state of the Application Site i.e. direct access to Lin Ma Hang Road with close proximity to Heung Yuen Wai BCP for transportation of construction materials to and from Mainland China and flat and formed land. The proposed development is considered fully commensurate with its local geographical setting and ideal to attain utmost land use maximization without inflicting undesirable impacts on the environment.

5.7 Not Incompatible with Surrounding Land Uses

The proposed development is considered not incompatible with the surrounding land uses. The surrounding of the Application Site mainly consists of scattered temporary structures, open storage and unused vacant land. A site of 122,819 sq.m. to the southeast of the Application Site for temporary logistic centre, warehouse (excluding dangerous goods godown) and container vehicle park with ancillary facilities was approved by the Board on 15.3.2024. Taking into consideration that the Applicant has proposed proper mitigation measures, including operation within the enclosed warehouse and restriction of operation hours, the proposed development could co-exist well with all these surrounding land uses without posing any adverse environmental impacts. As the proposed development is considered not incompatible with other developments/facilities in the adjacent areas in terms of nature and scale of use, approval of the application would therefore not result in any interface problems with the surrounding areas.

5.8 No Adverse Traffic Impact

The Application Site is directly accessible via Lin Ma Hang Road and no more than 3 round trips daily will be made by goods vehicles to/from the Application Site. Most of the vehicles will enter to/get out of the Application Site at non-peak hours (between 10am – 4pm). As such, it is anticipated that traffic generated by the proposed development to Lin Ma Hang Road will be very minimal and not be overloaded should the application be approved. In addition, sufficient spaces will be reserved at the southern portion of the Application Site for loading/unloading and vehicle manoeuvring. It is further envisaged and unlikely that the foregoing minimal volume of traffic generated from the proposed development would adversely affect the existing and/or planned road networks. There will also be no need for goods vehicles queuing up outside the Application Site under any circumstances.

5.9 No Adverse Environmental Impacts on the Surrounding Areas

As reiterated in Section 4.7, the proposed warehouse will only be used as storage of construction materials and all operations will be confined within the enclosed warehouse. No workshop activities will be undergone within the Application Site and relevant mitigation measures will be conducted to minimize all the potential environmental impacts on the surrounding areas. In addition, the operating hours will be restricted from 9am to 7pm, the proposed development will unlikely create significant adverse environmental impact to the surrounding areas.

Since the surrounding environments are characterized by various rural and storage uses, with the proposed storage use also being confined within the enclosed structure, no adverse environmental impacts on the surrounding area is anticipated.

5.10 No Adverse Drainage Impact

Drainage facilities will be provided within the Application Site for this application, surface runoff will be effectively collected from and discharge out of the Application Site. The Applicant has accordingly submitted a drainage proposal (see **Appendix E**) to elaborate. Therefore, no adverse drainage impact would be generated from the Application Site.

5.11 No Creation of Undesirable Precedent

Having realized the Board is entitled to consider planning applications, there may be little concern about setting an undesirable precedent by approving the current application. The proposed development is an exceptional case which is justified on the demand for relocation of warehouse due to land resumption to support the construction industry in Hong Kong. The proposed development at the Application Site would be able to support the construction industry, especially with the increasing development projects in Hong Kong in the coming years. Therefore, it is proposed that the permission is only given to the development under this application.

As the Board will consider each planning application on its individual merits, which in a sense, physical states of and surrounding the Application Site are always assessed individually and differently even if they are of similar nature. Approving the current application should therefore not entail to the approval of other similar applications under any circumstances.

6. Conclusion

The Application Site falls within “REC” zone on the approved Ta Kwu Ling North OZP. The Applicant is currently operating at Shan Ha Tsuen, Yuen Long, which is subject to land resumption for the Second Phase Development of Yuen Long South Development Area. As the Applicant is required to relocate before September 2024, they are in a dire need to search for a relocation site by then.

In facing difficulties in identifying a suitable site for relocation of their business, there is no other alternative option for the Applicant due to limited available site in the market. The Application Site is considered as the best location choice due to its locational advantage and readily availability for warehouse use. Valuable land resources can also be optimized due to its locational advantage and similar uses on the surrounding areas in which no interface problems will be arisen out of the proposed development with similar warehouse application recently approved in the vicinity. In view of the site location, surrounding land uses and the nature of the proposed development, the Application Site is considered suitable for proposed temporary warehouse for storage of construction materials.

Despite the fact that the Application Site fall within the “REC” zone, the planning intention of “REC” zone is hardly to be materialized with no long-term planned recreational development in the locality. In contrast, the proposed development in the interim at the Application Site is deemed not unsuitable for temporary uses with proposed mitigation measures and submission of relevant documents. Also, instead of the Application Site being not fully utilized, the proposed development encourages the optimization and revitalization of the land use of the Application Site whilst contributing to the construction industry in Hong Kong.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 3 years.

Lawson David & Sung Surveyors Ltd.
April 2024

Figures

- Figure 1 Location Plan
- Figure 2 Lot Index Plan
- Figure 3 Vehicular Access to the Site
- Figure 4 Indicative Layout Plan



擬議連接蓮塘/香園圍道路 (路線有待詳細設計)
 ED LIANTANG / HEUNG YUEN WAI CONNECTING ROAD
 (ALIGNMENT SUBJECT TO DETAILED DESIGN)

Application Site

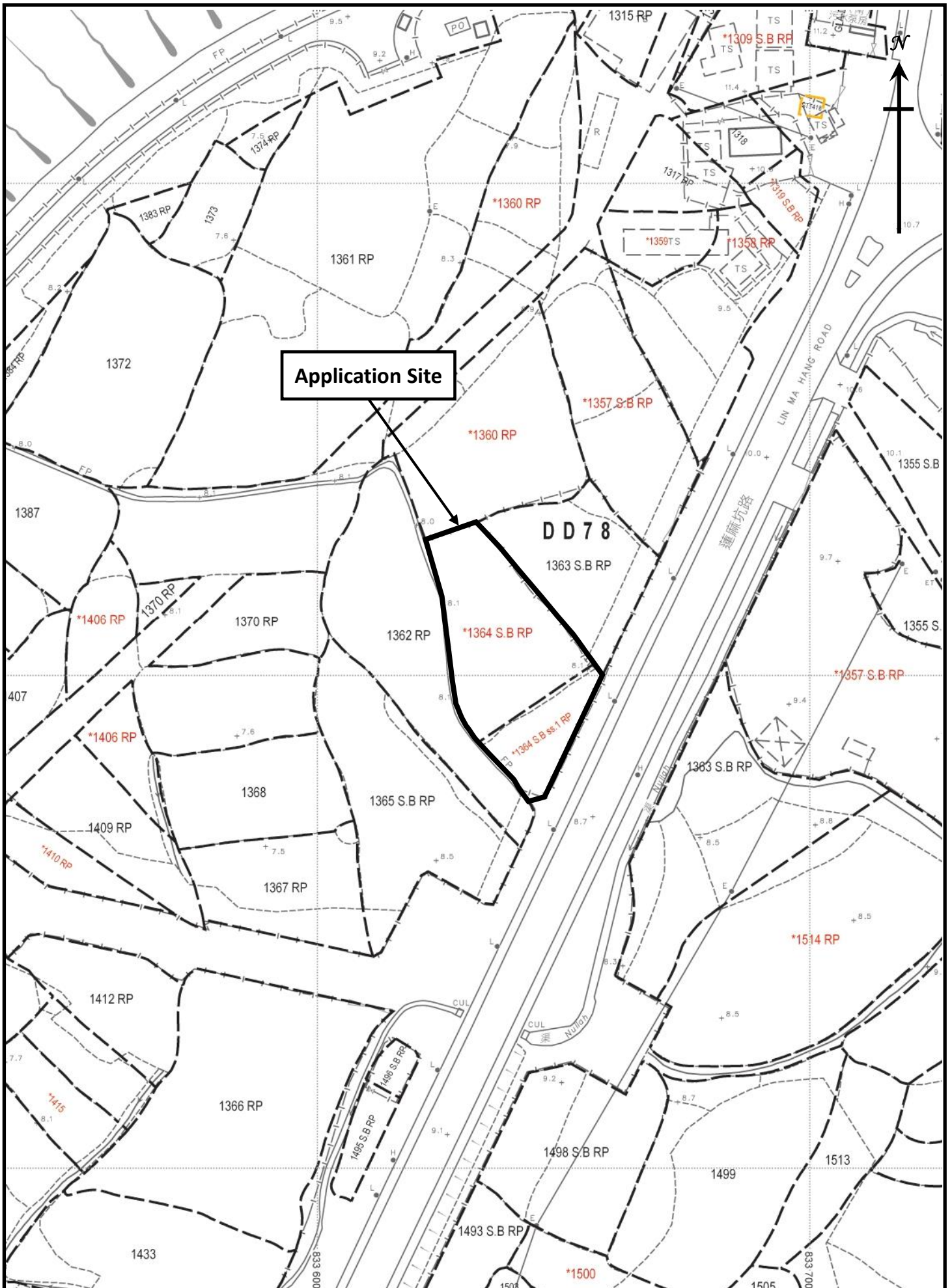
Location Plan
 (Extract from Approved Ta Kwu Ling North Outline
 Zoning Plan No. S/NE-TKLN/2)

Figure 1

1 : 7500



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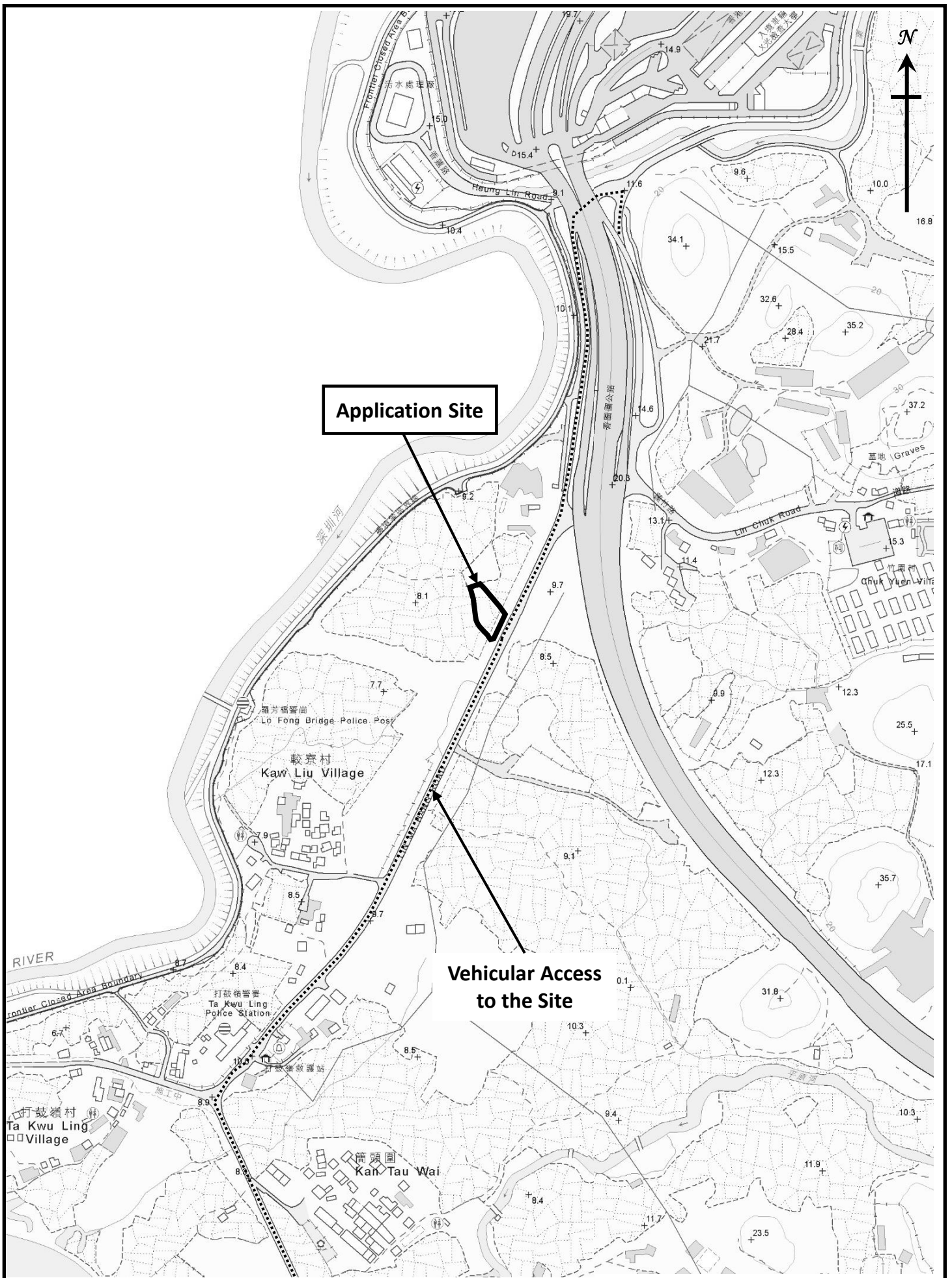
Lot Index Plan

Figure 2

1 : 1000



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 Estate Agency Licence No. C-006328



Vehicular Access to the Application Site

Figure 3

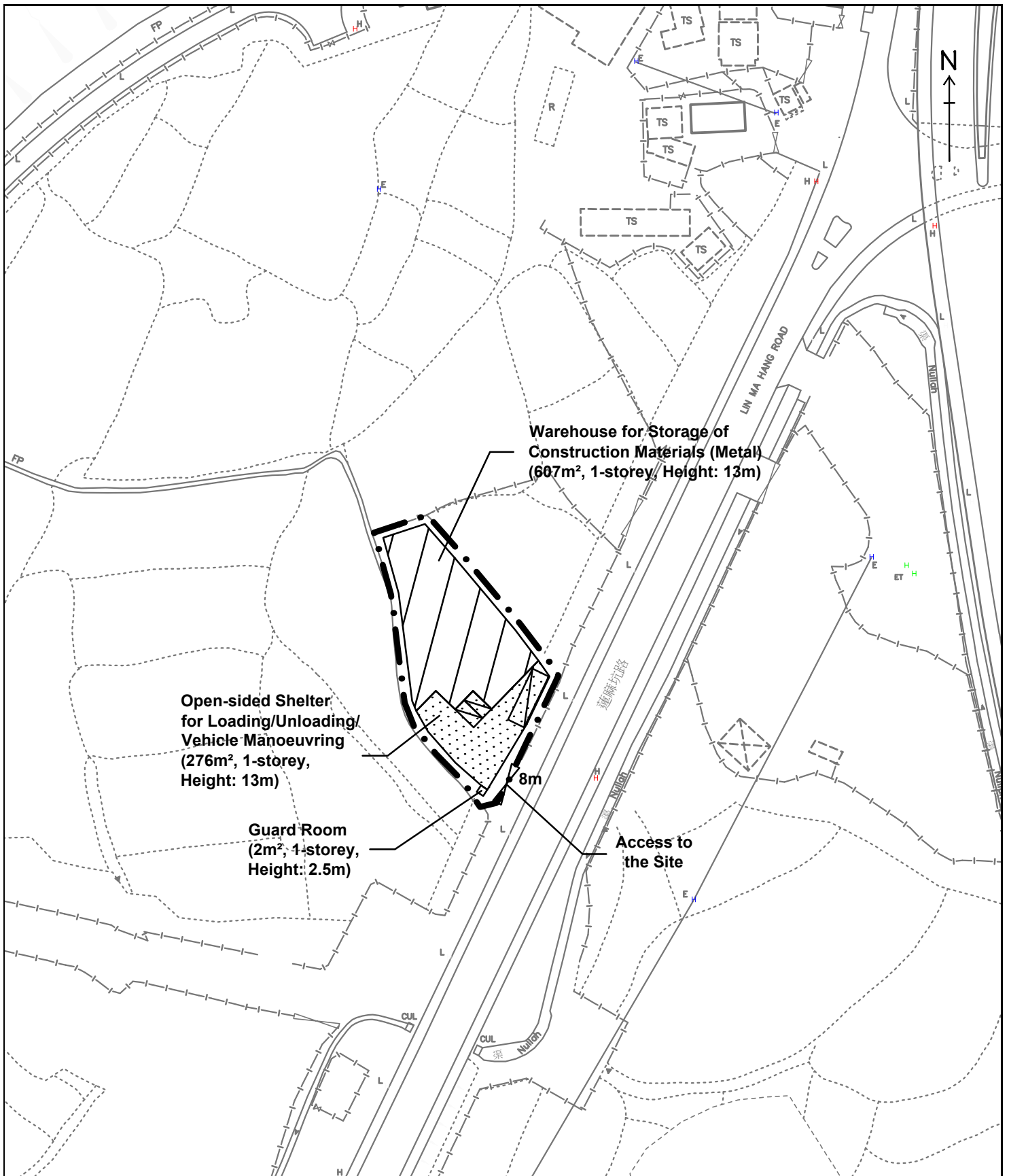
1 : 5000



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Legend:

- Application Site (about 1,105 sq.m.)
- Warehouse for Storage of Construction Materials (Metal)
- Loading/Unloading Space for Medium Goods Vehicle (3.5m x 1 1m) (1 no.)
- Parking Spaces for Staff/Visitors (2.5m x 5m) (2 nos.)

For Identification Purpose

Indicative Layout Plan

Figure 4

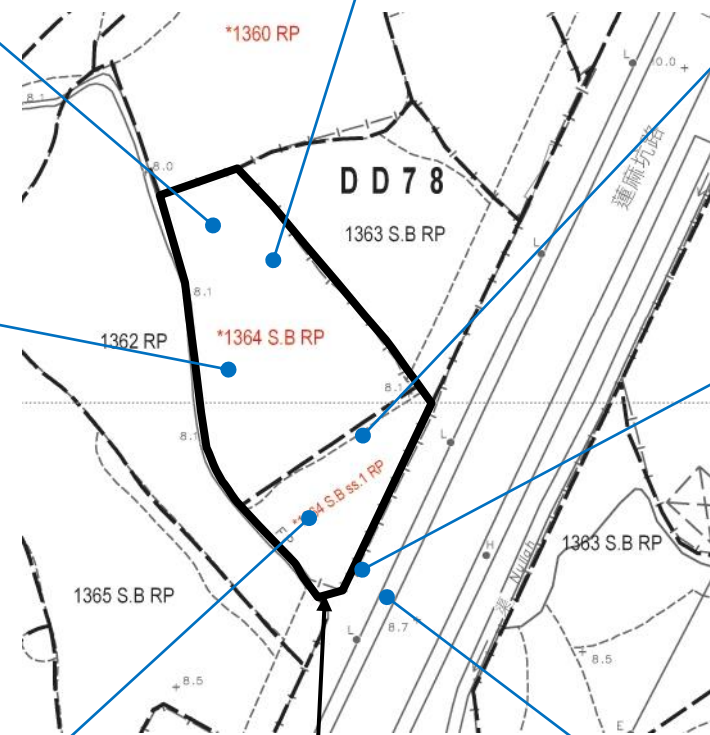
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Site Photos

Photo Plate 1 Existing Condition of the Application Site



Existing Condition of the Application Site

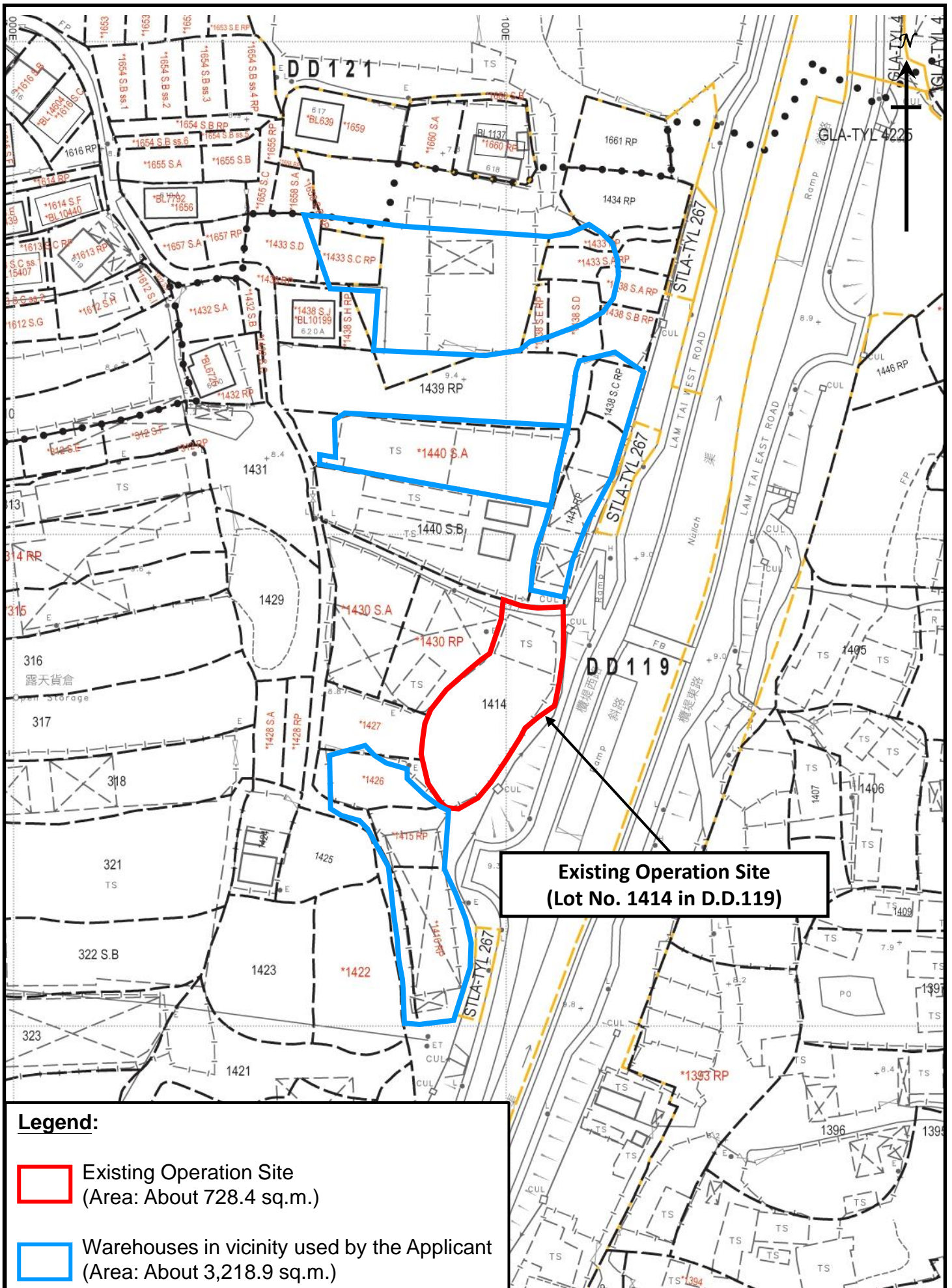
Photo Plate 1



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Appendix A

Location Plan of Existing Operation Site



Legend:

- Existing Operation Site
(Area: About 728.4 sq.m.)
- Warehouses in vicinity used by the Applicant
(Area: About 3,218.9 sq.m.)

Location Plan of Existing Operation Site

Appendix A

Not to scale



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Estate Agency Licence No. C-006328

Appendix B

Alternative Site Options for
Relocation of the Applicant's Existing Premises

Alternative Site Options for Relocation of the Applicant's Existing Premises

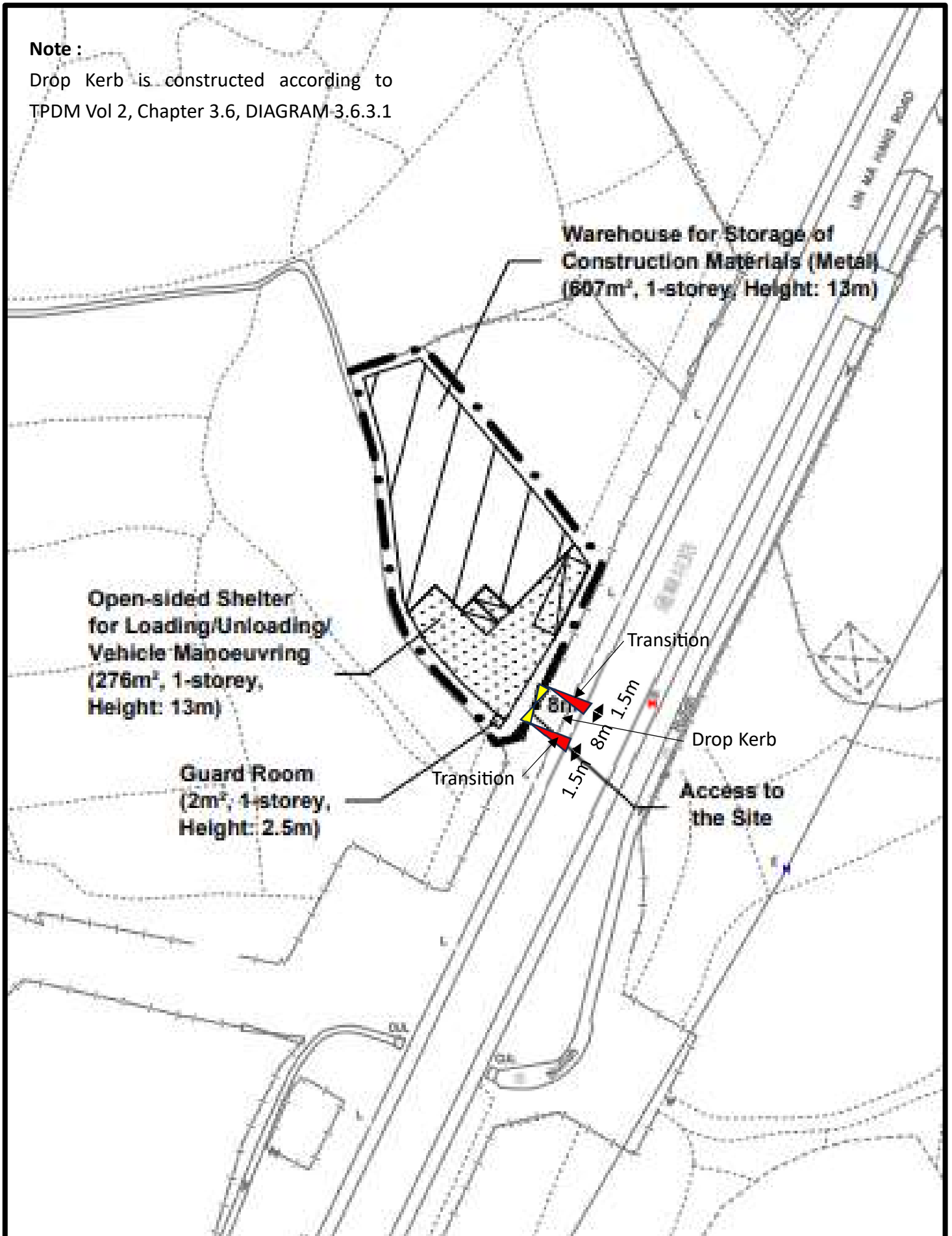
	Possible Site Location	Zoning	Site Area (m ²) (about)	Reasons of unsuitable for relocation
(a)	DD82 Lots 768 & 769 RP (Ta Kwu Ling, North District)	REC	4,220	<ul style="list-style-type: none"> ● Site located very close to nearby village settlements ● required clearance of vegetation with no direct access road.
(b)	DD100 Lot 1596 S.G RP (Kwu Tung South, Sheung Shui)	AGR	751	<ul style="list-style-type: none"> ● Site located very close to nearby village settlements and residential developments ● Required clearance of vegetation ● Narrow access road for goods vehicles
(c)	DD12 Lot 305 S.A (Part) (Tung Tsz Shan Road, Tai Po)	GB	946	<ul style="list-style-type: none"> ● Site access road trespassed various private lots and too narrow for goods vehicles ● No public transport facilities for staff
(d)	DD84 Lots 9 S.A, 10 S.A, 10 S.B and 11 (Part) (Ping Che, Ta Kwu Ling)	AGR	10,700	<ul style="list-style-type: none"> ● Site required clearance of vegetation ● No existing direct vehicular access ● Active agricultural activities are found on site

Appendix C

Drop Kerb Details and Typical Details of Run-in

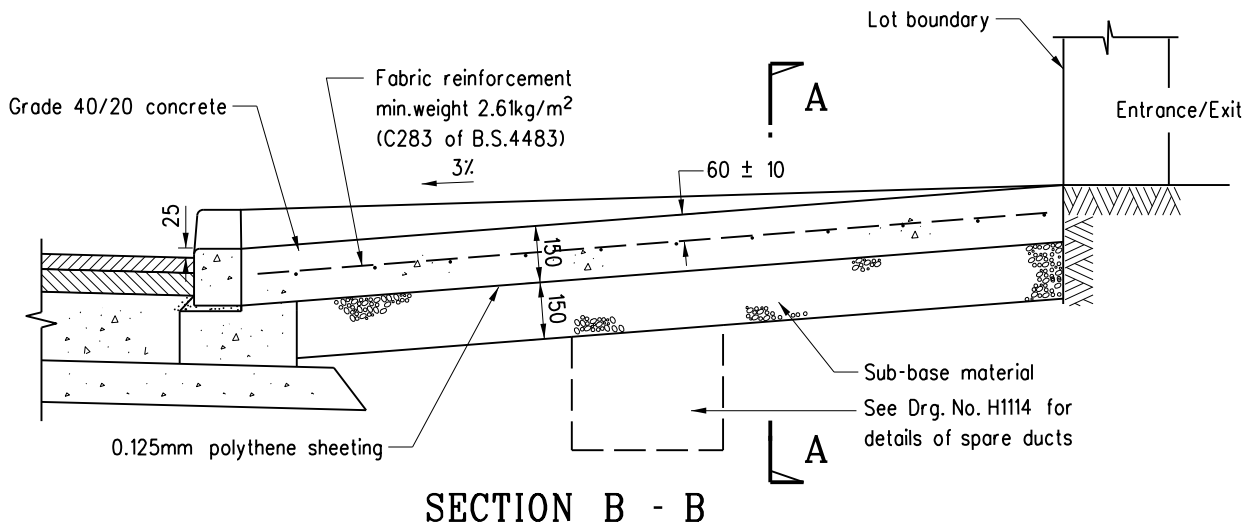
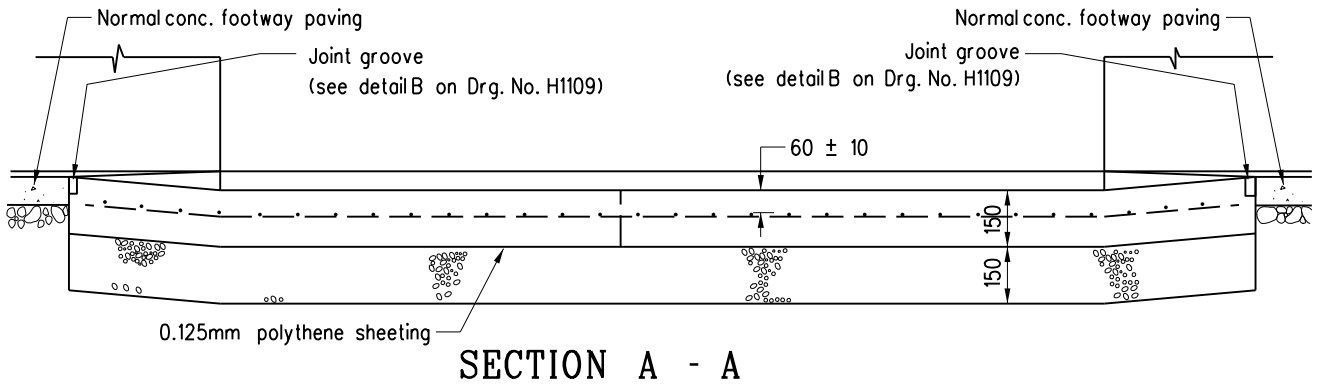
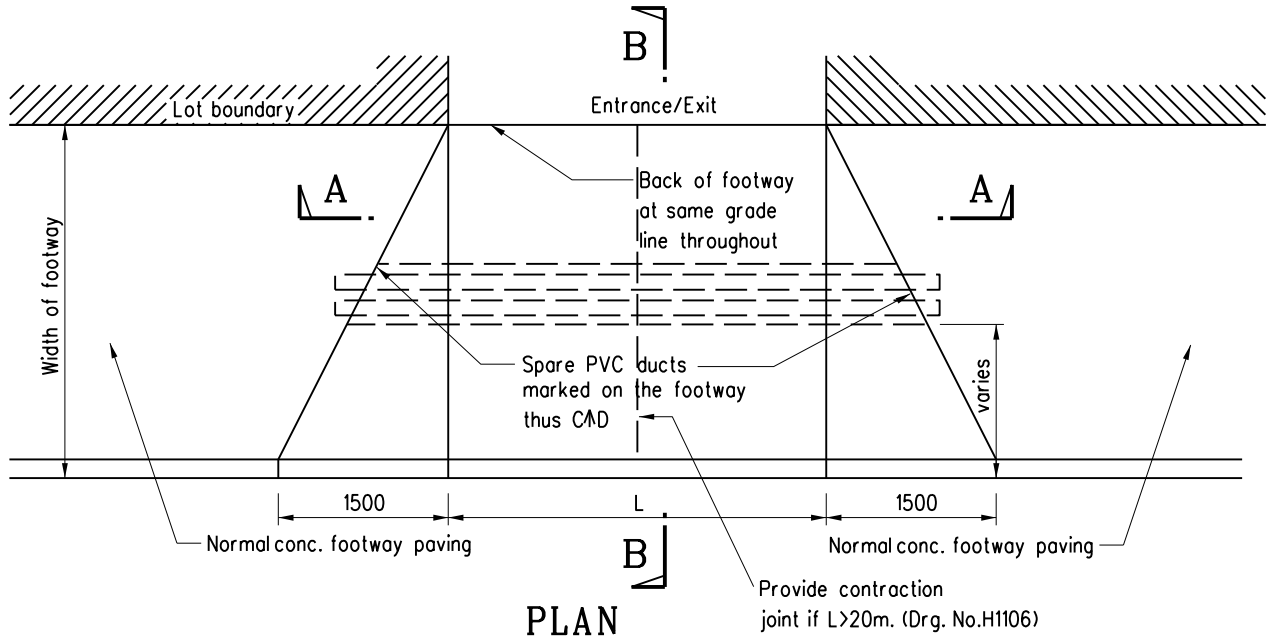
Note :

Drop Kerb is constructed according to TPDM Vol 2, Chapter 3.6, DIAGRAM 3.6.3.1



Drop Kerb Details

Drawing No. LMH 03



Note:

1. All dimensions are in millimetres.

REF.	REVISION	SIGNATURE	DATE
C	General revision	Original signed	Oct 19
B	Joint groove details revised	-	Nov 96
A	Grade of concrete revised	-	Sept 96
	Former Drg. No. H1011A with general revision	-	June 94

**TYPICAL DETAILS
OF RUN-IN
(SHEET 1 OF 2)**

HIGHWAYS DEPARTMENT

REFERENCE

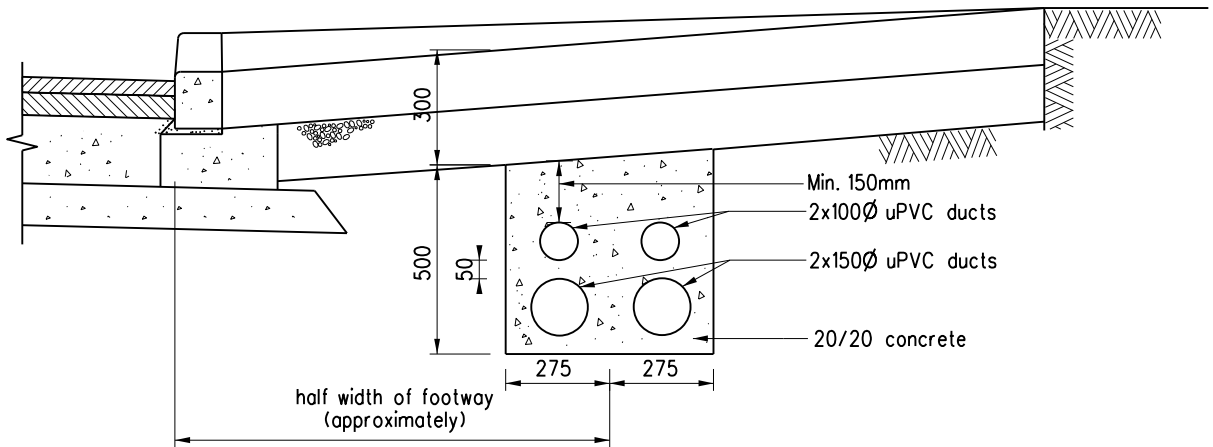
DRAWING No.

CAD

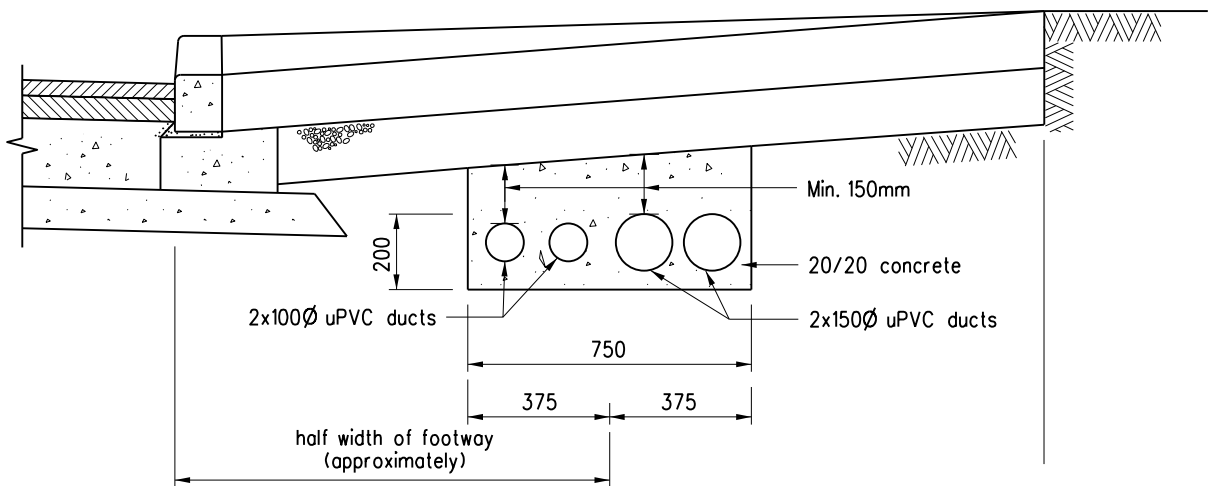
SCALE

1:20

H 1113C



OPTION A



OPTION B

Notes:

1. 100 diameter ducts are provided for cables of ATC or CCTV.
150 diameter ducts are provided for power cables.
2. The choice of option depends on the site situations (e.g. width of footway, existing underground utilities).
3. Position of both ends of the duct bank to be marked on footway thus CAD.

B	General revision	Original signed	Oct 19
A	Concrete cover revised		Sep 96
	Former Drg. No. H1011A with general revision		Jun 94
REF.	REVISION	SIGNATURE	DATE

TYPICAL DETAILS
OF RUN-IN
(SHEET 2 OF 2)

HIGHWAYS DEPARTMENT

REFERENCE

DRAWING No.

CAD

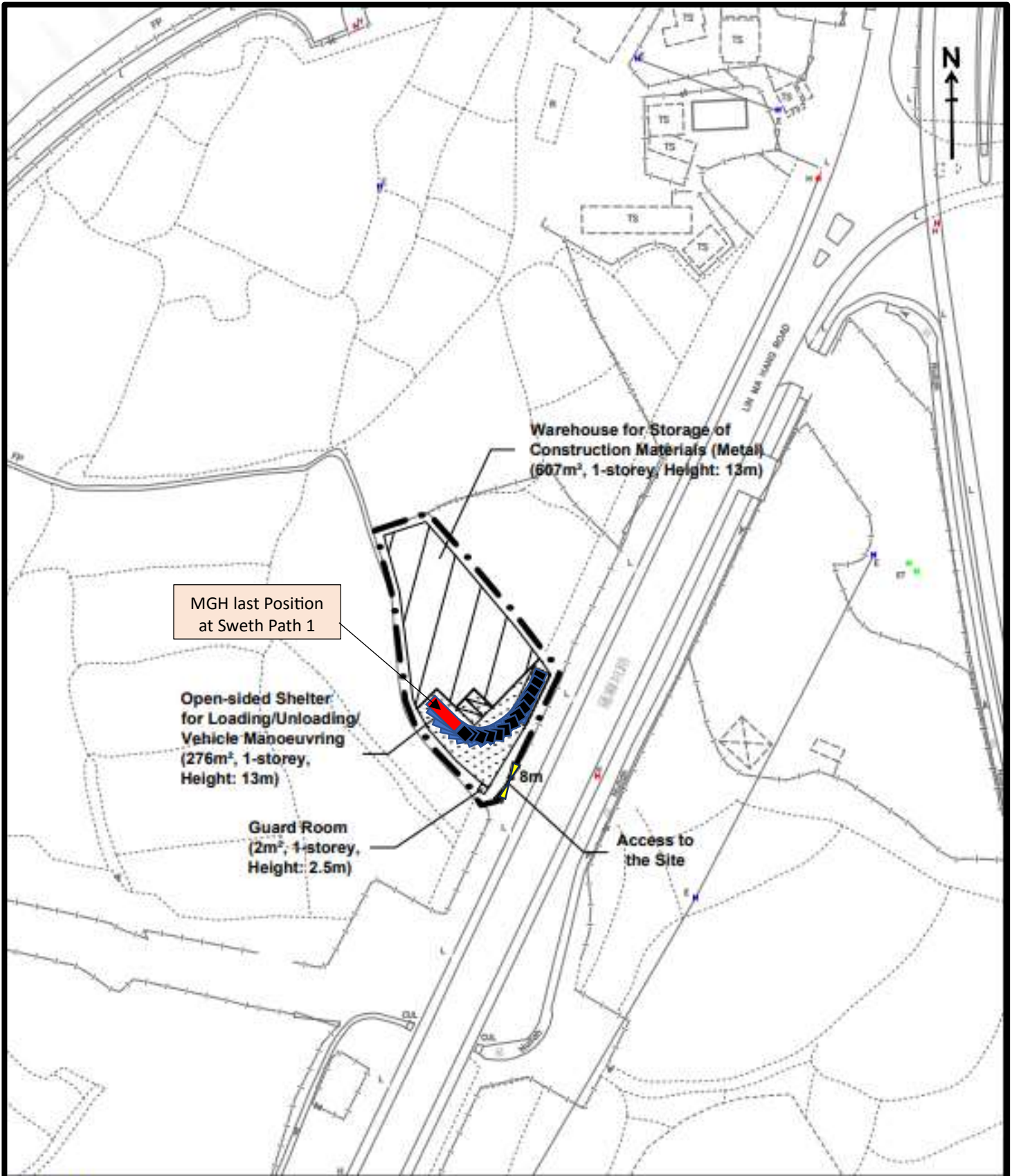
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H 1114B

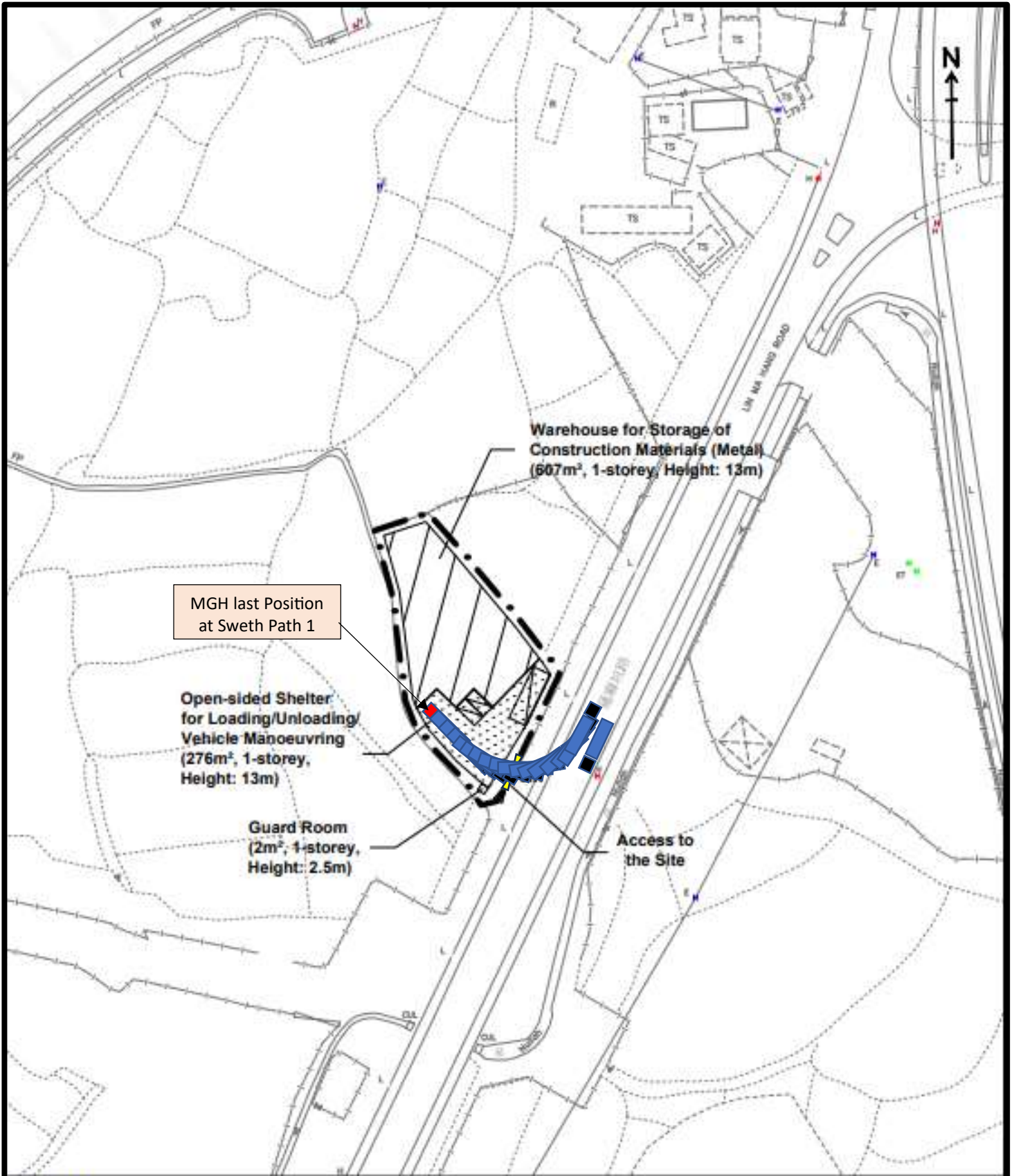
Appendix D

Swept Path Analysis






Legend:

- Application Site (about 1,105 sq.m.)
- Warehouse for Storage of Construction Materials (Metal)
- Loading/Unloading Space for Medium Goods Vehicle (3.5m x 11m) (1 no.)
- Parking Spaces for Staff/Visitors (2.5m x 5m) (2 nos.)
- For Identification Purpose



Legend:

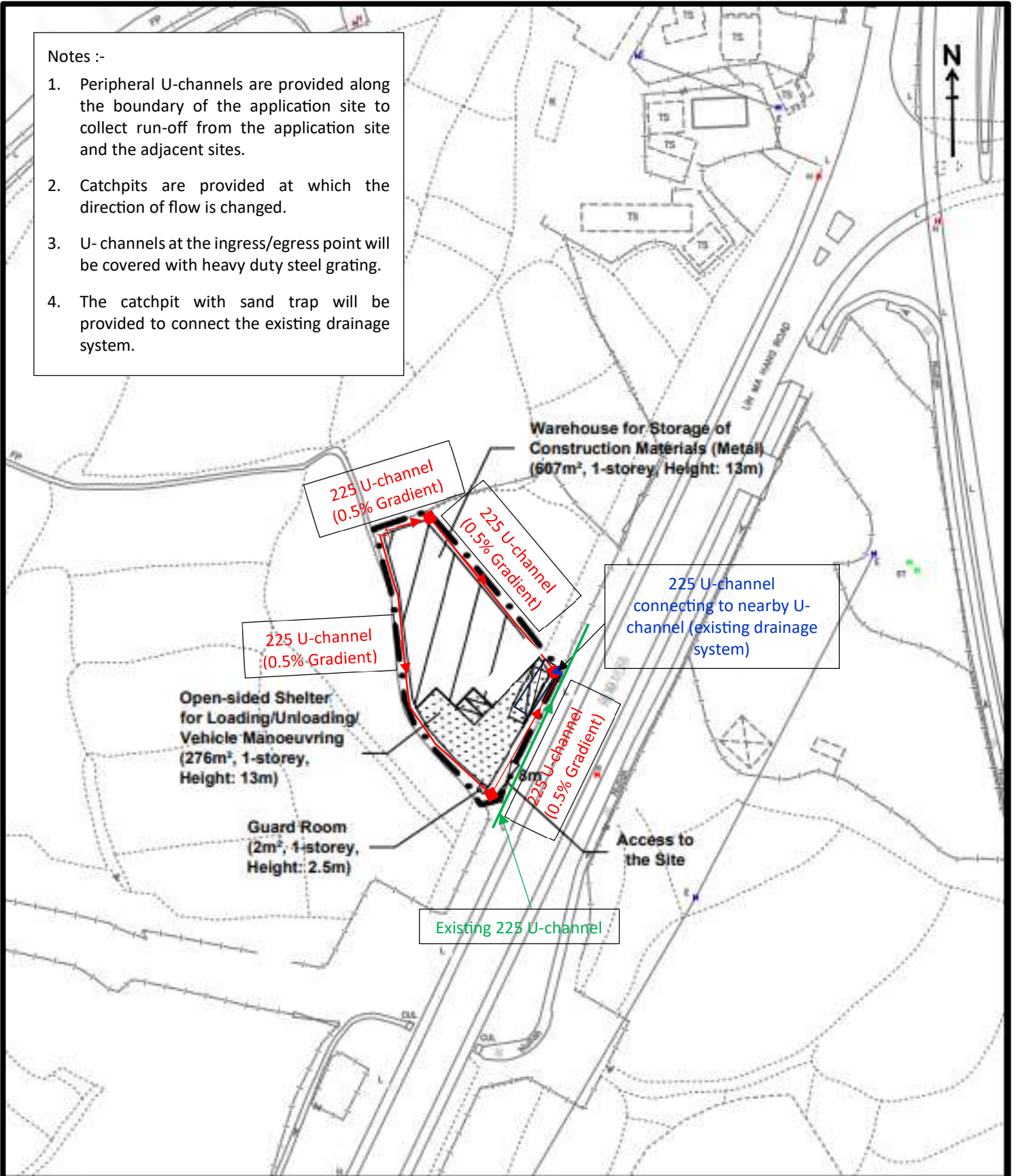
-  Application Site (about 1,105 sq.m.)
 -  Warehouse for Storage of Construction Materials (Metal)
 -  Loading/Unloading Space for Medium Goods Vehicle (3.5m x 11m) (1 no.)
 -  Parking Spaces for Staff/Visitors (2.5m x 5m) (2 nos.)
-  For Identification Purpose

Appendix E

Drainage Proposal

Notes :-

1. Peripheral U-channels are provided along the boundary of the application site to collect run-off from the application site and the adjacent sites.
2. Catchpits are provided at which the direction of flow is changed.
3. U-channels at the ingress/egress point will be covered with heavy duty steel grating.
4. The catchpit with sand trap will be provided to connect the existing drainage system.



Legend:

-  Application Site (about 1,105 sq.m.)
-  Warehouse for Storage of Construction Materials (Metal)
-  Loading/Unloading Space for Medium Goods Vehicle (3.5m x 11m) (1 no.)
-  Parking Spaces for Staff/Visitors (2.5m x 5m) (2 nos.)

For Identification Purpose

Drainage Plan

Drawing No. LMH 04

Drainage Design for the site at DD78

Lin Ma Hang Road

DSD - STORMWATER DRAINAGE MANUAL

7.5.2 Rational Method

$$Q_p = 0.278CiA$$

where Q_p = peak runoff in m^3/s

C = runoff coefficient (dimensionless)

i = rainfall intensity in mm/hr

A = catchment area in km^2

In Hong Kong, a value of $C = 1.0$ is commonly used in developed urban areas. In less developed areas, appropriate C values in order to ensure that the design would be fully cost-effective.

Surface Characteristics Runoff coefficient, C^*

Asphalt	0.70 - 0.95
Concrete	0.80 - 0.95
Brick	0.70 - 0.85
Grassland (heavy soil**)	
Flat	0.13 - 0.25
Steep	0.25 - 0.35
Grassland (sandy soil)	
Flat	0.05 - 0.15
Steep	0.15 - 0.20

The surface of the site will be covered by Asphalt, the C should be **0.85** (Mid value)

6.6.1 Village Drainage and Main Rural Catchment Drainage Channels

'Village Drainage' refers to the local stormwater drainage system within a village. A stormwater drain conveying stormwater runoff from an upstream catchment but happens to pass through a village may need to be considered as either a 'Main Rural Catchment Drainage Channel' or 'Village Drainage', depending on the nature and size of the upstream catchment. In any case, the impact of a 50-year event should be assessed in the planning and design of village drainage system to check whether a higher standard than 10 years is justified. **50 Years** is used.

Table 2d – Intensity-Duration-Frequency (IDF) Relationship of North District Area for durations not exceeding 240 minutes

Duration (min)	Extreme Intensity x (mm/h) for various Return Periods						
	T(year)						
	2	5	10	20	50	100	200
240	28.5	37.7	43.4	48.6	54.9	59.4	63.6
120	42.2	54.7	62.5	69.6	78.4	84.7	90.8
60	61.0	75.7	84.3	92.0	101	108	114
30	84.0	100	110	118	128	135	142
15	106	127	139	150	163	173	182
10	119	141	155	168	184	196	208
5	138	161	177	193	216	234	254

i (rainfall intensity) = 101mm/hr (Duration of 60min is used)

$$Q_p = 0.278CiA$$

C = 0.85 (Asphalt)(mid value) (Application Site)

C = 0.15 (Grass Land (Sandy Soil) (Adjacent Area)

i = 92 mm/hr

A = 1,105² (0.001105km²) (Application Site)

+ 1,105m² (0.001105km²) (Adjacent Area = 100% of Application Site)

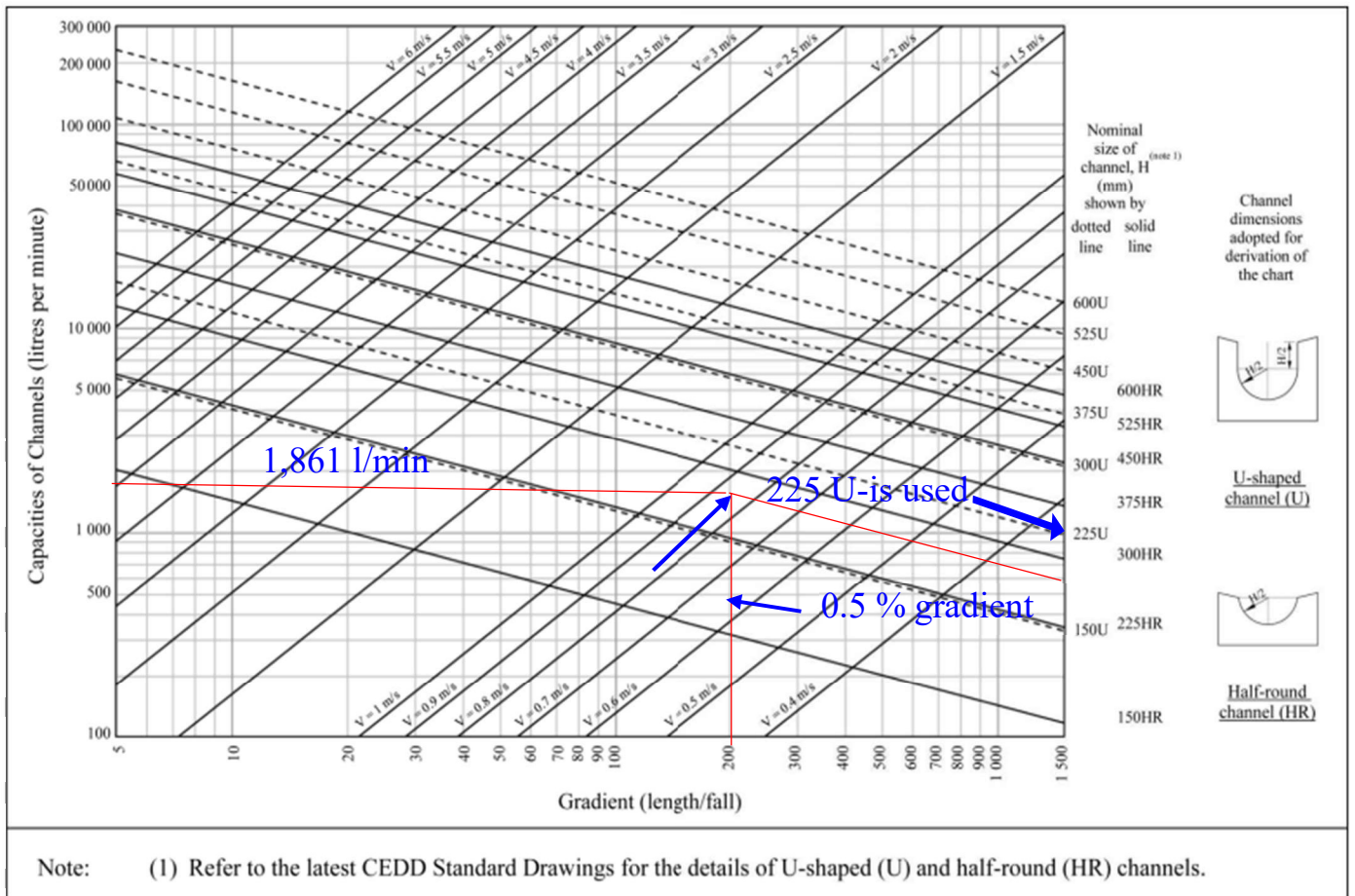
$$Q_p = 0.278 \times 101 \times ((0.85 \times 0.001105) + (0.15 \times 0.001105))$$

$$Q_p = 0.0310 \text{ m}^3/\text{s} \text{ or } 1,861 \text{ l/min}$$

For conservative calculations, all catchment areas are combined for all U-Channels.

GEO Technical Guidance Note No. 43 (TGN 43) Guidelines on Hydraulic Design of U-shaped

Figure 1 - Chart for the rapid design of U-shaped and half-round channels up to 600 mm



For 1,861 l/min, 225 U-channel is used.

Appendix F

Fire Service Installations Proposal

F.S.NOTES:

1. GENERAL

- 1.1 FIRE SERVICE INSTALLATIONS SHALL BE PROVIDED IN ACCORDANCE WITH THE CODES OF PRACTICE FOR MINIMUM FIRE SERVICE INSTALLATIONS AND EQUIPMENT AND INSPECTION, TESTING AND MAINTENANCE OF INSTALLATIONS AND EQUIPMENT 2022 (COP 2022), FSD CIRCULAR LETTERS AND THE HONG KONG WATERWORKS STANDARD REQUIREMENTS.
- 1.2 ALL TUBES AND FITTINGS SHALL BE G.M.S. TO BS1387 MEDIUM GRADE WHERE PIPEWORK UP TO $\phi 150\text{mm}$.
- 1.3 ALL TUBES AND FITTINGS SHALL BE DUCTILE IRON TO BS EN545 K12 WHERE PIPEWORK ABOVE $\phi 150\text{mm}$.
- 1.4 ALL DRAIN PIPES SHALL BE DISCHARGED TO A CONSPICUOUS POSITION WITHOUT THE POSSIBILITY OF BEING SUBMERGED.
- 1.5 ALL PUDDLE FLANGES SHALL BE MADE OF DUCTILE IRON
- 1.6 THE AGGREGATE AREA OF OPENABLE WINDOWS NOT LESS THAN 6.25% OF THE FLOOR AREA OF THE STRUCTURE
- 1.7 VENTILATION/AIR CONDITIONING SYSTEM NOT TO BE PROVIDED.

2. HOSE REEL SYSTEM

- 2.1 NEW FIRE HOSE REEL SHALL BE PROVIDED AS INDICATED ON PLAN TO ENSURE THAT EVERY PART OF THE BUILDING CAN BE REACHED BY A LENGTH OF NOT MORE THAN 30m HOSE REEL TUBING.
- 2.2 THE WATER SUPPLY FOR HOSE REEL SYSTEM WILL BE FED FROM A NEW 2m^3 F.S. FIBREGLASS WATER TANK VIA TWO HOSE REEL PUMPS (DUTY/STANDBY) LOCATED INSIDE FS PUMP ROOM AT EXTERNAL AREA.
- 2.3 HOSE REEL PUMPS SHALL BE STARTED BY ACTUATION OF ANY BREAKGLASS UNIT FITTED ASIDE EACH HOSE REEL SETS
- 2.4 ALL FIRE HOSE REEL OUTLETS SHOULD BE HOUSED IN GLASS FRONTED CABINET SECURED UNDER LOCK & KEY.
- 2.5 ALL FIRE HOSE REEL SHOULD BE PROVIDED WITH FSD APPROVED TYPE INSTRUCTION PLATE & WSD WARNING PLATE
- 2.6 SECONDARY ELECTRICITY SUPPLY DIRECTLY TEE OFF BEFORE CLP'S INCOMING MAIN SWITCH SHALL BE PROVIDED FOR THE FS PUMPS.

3. AUTOMATIC SPRINKLER SYSTEM

- 3.1 NEW AUTOMATIC SPRINKLER SYSTEM SHALL BE PROVIDED AND INSTALLED IN ACCORDANCE WITH LPC RULES FOR AUTOMATIC SPRINKLER INSTALLATIONS INCORPORATING BS EN 12845: 2015 (INCLUDING TECHNICAL BULLETINS, NOTES, COMMENTARY AND RECOMMENDATIONS) AND FSD CIRCULAR LETTER NO. 5/2020. THE CLASSIFICATION OF THE OCCUPANCIES WILL BE ORDINARY HAZARD GROUP III.
- 3.2 ONE NEW 135m^3 SPRINKLER WATER TANK WILL BE PROVIDED AS INDICATED ON PLAN. THE TOWN MAIN WATER SUPPLY WILL BE FED FROM SINGLE END.
- 3.3 TWO NEW SPRINKLER PUMPS (DUTY/STANDBY) AND ONE JOCKEY PUMP SHALL BE PROVIDED IN FS PUMP ROOM LOCATED AT EXTERNAL AREA.
- 3.4 NEW SPRINKLER CONTROL VALVE SET AND SPRINKLER INLET SHALL BE PROVIDED AS INDICATED ON PLAN.
- 3.5 A TEST VALVE SHALL BE PROVIDED FOR EACH ZONE OF SPRINKLER PIPE. THIS VALVE SHALL BE AT A CONSPICUOUS POSITION THAT WATER CAN BE DRAINED AWAY EASILY.
- 3.6 ALL SUBSIDIARY STOP VALVES TO BE ELECTRIC MONITORING TYPE.
- 3.7 ALL ELECTRIC TYPE VALVES SHOULD GIVE VISUAL SIGNALS TO FIRE SERVICE MAIN SUPERVISORY CONTROL PANEL TO INDICATE THE STATUS (OPEN/CLOSE) OF THE VALVES.
- 3.8 SECONDARY ELECTRICITY SUPPLY DIRECTLY TEE OFF BEFORE CLP'S INCOMING MAIN SWITCH SHALL BE PROVIDED FOR THE SPRINKLER PUMPS.
- 3.9 THE SPRINKLER SYSTEM DESIGN IS BASED ON THE FOLLOWINGS:
HAZARD CLASS : ORDINARY HAZARD GROUP III
TYPE OF STORAGE : POST-PALLET (ST2)
STORAGE CATEGORY : CATEGORY I
MAXIMUM STORAGE HEIGHT : 3.5m
SPRINKLER PROTECTION : CEILING PROTECTION ONLY

4. FIRE ALARM SYSTEM

- 4.1 NEW FIRE ALARM SYSTEM SHALL BE PROVIDED IN ACCORDANCE WITH BS 5839 PART 1: 2017 AND FSD CIRCULAR LETTERS 6/2021
- 4.2 NEW BREAKGLASS UNITS AND FIRE ALARM BELLS SHALL BE PROVIDED AT ALL NEW FIRE HOSE REEL POINTS. THE FIRE ALARM INTALLATION WILL BE INTEGRATED WITH THE HOSE REEL SYSTEM.

5. EMERGENCY LIGHTING

- 5.1 EMERGENCY LIGHTING SHALL BE PROVIDED IN ACCORDANCE WITH 'BS 5266-PART 1 :2016 AND BS EN 1838 :2013', FSD CIRCULAR LETTER 4/2021, COVERING ALL AREA. EMERGENCY LIGHTINGS SHALL BE BACKED UP BY BUILT-IN BATTERY AND CAPABLE OF MAINTAINING FUNCTION OF NOT LESS THAN 2 HOURS IN CASE OF POWER FAILURE

6. EXIT SIGN

- 6.1 ALL EXIT SIGNS/DIRECTIONAL EXIT SIGNS SHALL BE PROVIDED IN ACCORDANCE WITH 'BS 5266-PART 1 :2016 AND FSD CIRCULAR LETTER NO. 5/2008, FOR THE BUILDING. EXIT SIGNS/DIRECTIONAL EXIT SIGNS SHALL BE BACKED UP BY BUILT-IN BATTERY AND CAPABLE OF MAINTAINING FUNCTION OF NOT LESS THAN 2 HOURS IN CASE OF POWER FAILURE.

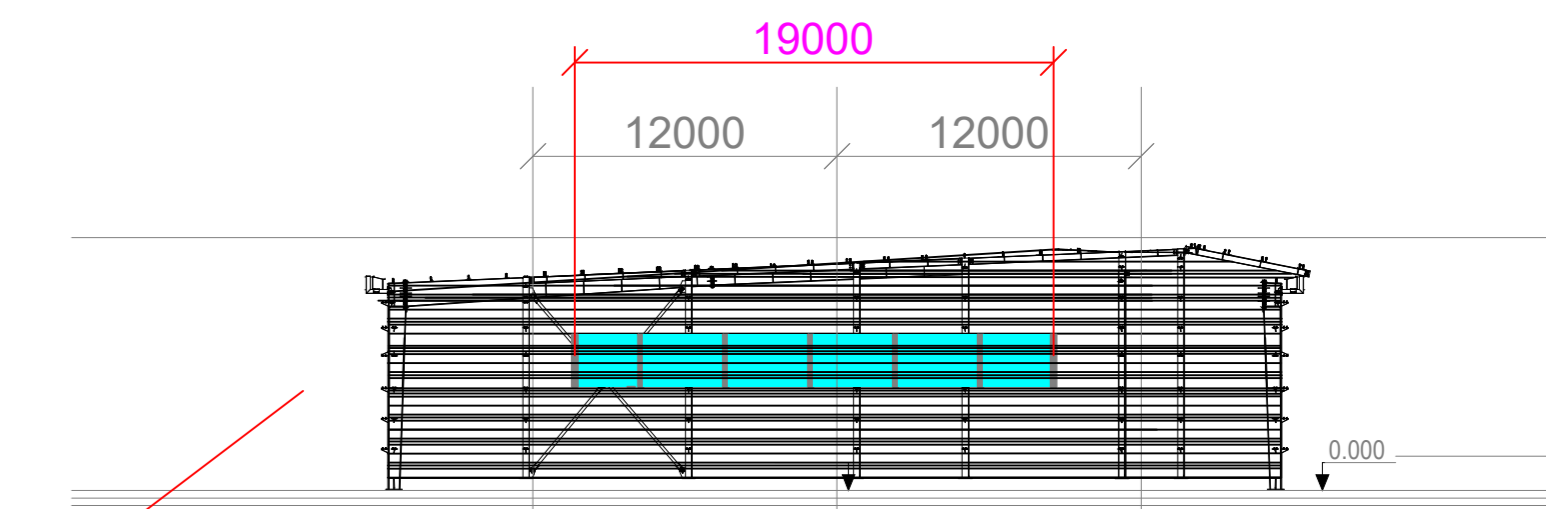
7. PORTABLE APPLIANCES

- 7.1 PORTABLE HAND OPERATED APPLIANCES SHALL BE PROVIDED AS INDICATED ON PLAN.

LEGEND

HOSE REEL	EMERGENCY LIGHT	5KG CO2 FIRE EXTINGUISHER	5KG DRY POWDER FIRE EXTINGUISHER
BREAK GLASS UNIT	EXIT SIGN	SPRINKLER CONTROL VALVE SET	SPRINKLER HEAD (ON PLAN)
FIRE ALARM BELL	SUBSIDIARY VALVE / FLOW SWITCH	SPRINKLER INLET	PUMP SET

Structure 1 Openable Windows Calculation
 Area of Structure B1 = 607sq.m.
 Area of High Bay Window (H.B.W.) = 2.0m(H) x 19m = 38 sq.m.
 Total openable window area = 38 sq.m.
 = 6.26% of floor area



Section drawing of window opening for the structure 1

Structure 1
Warehouse for Storage of
Construction Materials (Metal)
(607m², 1-storey, Height: 13m)

Open-sided Shelter
for Loading/Unloading/
Vehicle Manoeuvring
(276m², 1-storey,
Height: 13m)

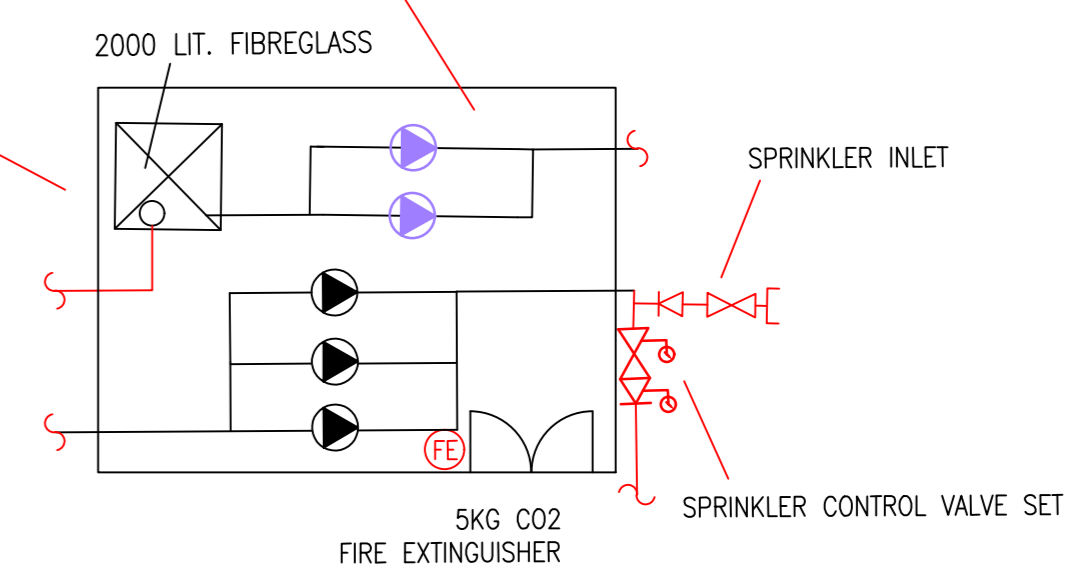
Guard Room
(2m², 1-storey,
Height: 2.5m)



135000 LIT. RCC SPRINKLER WATER TANK (UNDER GROUND)

Water Tank Room For FSIs

Pump Room For FSIs



Access to the Site

PROJECT : Indicative Layout Plan	DRAWING TITLE : F.S. Notes, Legend, Fire Service Installation Layout Plan	ARCHITECT :	CONSULTANT :	FIRE SERVICE CONTRACTOR : Century Fire Service Engineering Co., Ltd.	NAME : C.K.NG	DATE : 22 MAR 2024	DRAWING NO : FS-01	REV. : 0
					DRAWN BY :		SCALE : 1 : 300 (A0)	
					CHECKED BY :		SOURCE : B.O.O. Ref. BD F.S.D. Ref. FP	
					APPROVED BY :			
		REV	DESCRIPTION	DATE				